



Environmental Compliance and Enforcement
Environmental Stewardship Division
Environment and Climate Change
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January 12, 2026
Client File No.: 1069.10, 1071.10, 53.110
Our File Nos.: 020-17-08-11-00
020-17-08-11-0N

Attention: Yvonne Hawryliuk, Director

RE: 2024 Annual River Crossing Report

Further to your December 24, 2025 letter, below are the clarifications requested related to the 2024 Annual River Crossing Report.

- ***In Figure 2, what is the interpretation of a crossing having a condition grade of zero? Also, how do you define the “consequence” axis?***

Figure 2 was from the High-Risk River Crossings Phase 3 report. The condition grade of 0 was assigned to 5 sites indicating they were abandoned in place.

Since this report, improvements have been made for the River Crossings Asset Management Plan (AMP), which will be forwarded to your office in Q1 2026. The AMP changed the way this figure was depicted, such as excluding abandoned pipes that are no longer transporting wastewater. See below for further details on each of the 5 pipes listed with a condition of 0:

- Site 9: Frasers Grove Park Pipe S-MA70021128 was abandoned in 2023. This is noted on the asset card.
- Site 16: Fort Garry Cemetery S-MA70003330 was abandoned in place and requires removal. This is noted on the asset card. There is capital budget in 2026 for this work to be completed.
- Site 17 (two): Fort Garry Bridge - Assigned condition grade of 0 as it is no longer in use. The pipe was removed in March 2025. Wastewater is currently crossing the river via a bypass arrangement over the bridge. Construction of two new river crossings is underway, with expected substantial completion in early Q2 2026. Upon commissioning of the new crossing pipes, they will be assigned a condition of 1, or Very Good.
- Site 20: Omands Creek S-MA70031433 was abandoned. This is noted on the asset card.

The consequence axis was determined as part of the 2006 Wastewater Sewer River Crossing Risk Assessment report (summarized in section 4.0). Attributes such as pipe material, pipe diameter, crossing type (force main, bridge, siphon or gravity), stream classification and number of pipes were considered in determining this value.

- ***The conclusion of the report indicated that West Perimeter Bridge will be rehabilitate in the next 5 years, yet the table in the asset card shows no 3-5 year funding for that asset.***

The West Perimeter Bridge Asset Card has been edited to include the information required. A consulting design contract will be issued in 2026, with construction in 2027/2028. This will be updated in the 2025 report.

- ***You refer to “Old Crescent Drive” in the conclusion of the report, but there is only a “Crescent Drive” asset card. Are these the same? If so, there is no capital works funding listed in the table of that asset card, when the conclusion of the report stated it will be rehabilitated in the next five years.***

The Fort Garry Cemetery Asset Card has been edited to include the alternative name of "Old Crescent Drive" and the rehabilitation details. A consulting design contract will be issued in 2026 with construction work to follow. This will be updated in the 2025 report.

- ***Some fields in the asset cards are blank including remaining service life, visual inspection schedule, and isolation valve exercising. Why is this information not included?***

Missing data types are addressed as follows:

- a. Remaining service life calculations and estimated replacement costs were completed as part of the River Crossings AMP and will be included in the 2025 report.
- b. Isolation valve and inspection data updates were an improvement recommendation as part of the River Crossings AMP.

The additional clarifications requested to the 2023 Annual River Crossing Report are as follows:

- ***How inspection frequencies are determined and why they deviate from the approved 2017 Technical Memorandum.***

In 2025, the City commissioned a comprehensive AMP for the River Crossing asset class. Section 4.3.3 from the draft AMP which outlines how inspection frequencies are determined is provided in the appendix – Figure 1. As noted above, the AMP will be forwarded to your office shortly.

The 2017 Technical Memo differs as it was related to a specific project's requirements rather than a comprehensive asset management strategy. The 2017 memo outlined a plan to meet licensing requirements for upgrading the Northeast Interceptor and wasn't intended to address the overall inventory of river crossing assets. The frequency table, while relevant in 2017, cited information from 2001. This information came before the City developed a strategy for classifying and monitoring river crossings. Since 2017, the City has gained more knowledge about deterioration and uses advanced analytics and tools to assess the condition and quantify failure risk of the river crossing assets.

- ***Identifying plans for inoperable valves***

The asset cards list some inoperable isolation valves within the immediate area of certain river crossings. Despite this, the City still has the ability to isolate all crossings either by operating a valve further upstream/downstream of the inoperable valve or by using a more reliable form of isolation, such as an inflatable plug within the pipe.

The AMP identifies documentation corrections as a plan improvement activity the City can consider. For example, in some cases the asset card lists inoperable isolation valves that have either been replaced or are no longer required but haven't been taken out of the asset inventory. The City will undertake these corrections, along with other recommended actions, on a risk-based priority for completion.

The additional changes requested for the 2025 Annual River Crossing Report will be incorporated in all reports going forward.

Should you have any further questions, please contact me at 204-986-5210 or by email at cwiebe@winnipeg.ca.

Sincerely,



Cynthia D. Wiebe, P. Eng., CAMP
Manager of Engineering Services

Attachment

CW/jkm

- c: Y. Hawryliuk, Environment and Climate Change (email)
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- N. Rentz, Environment and Climate Change (email)
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Figure 1: High Risk River Crossing AMP – Section 4.3.3

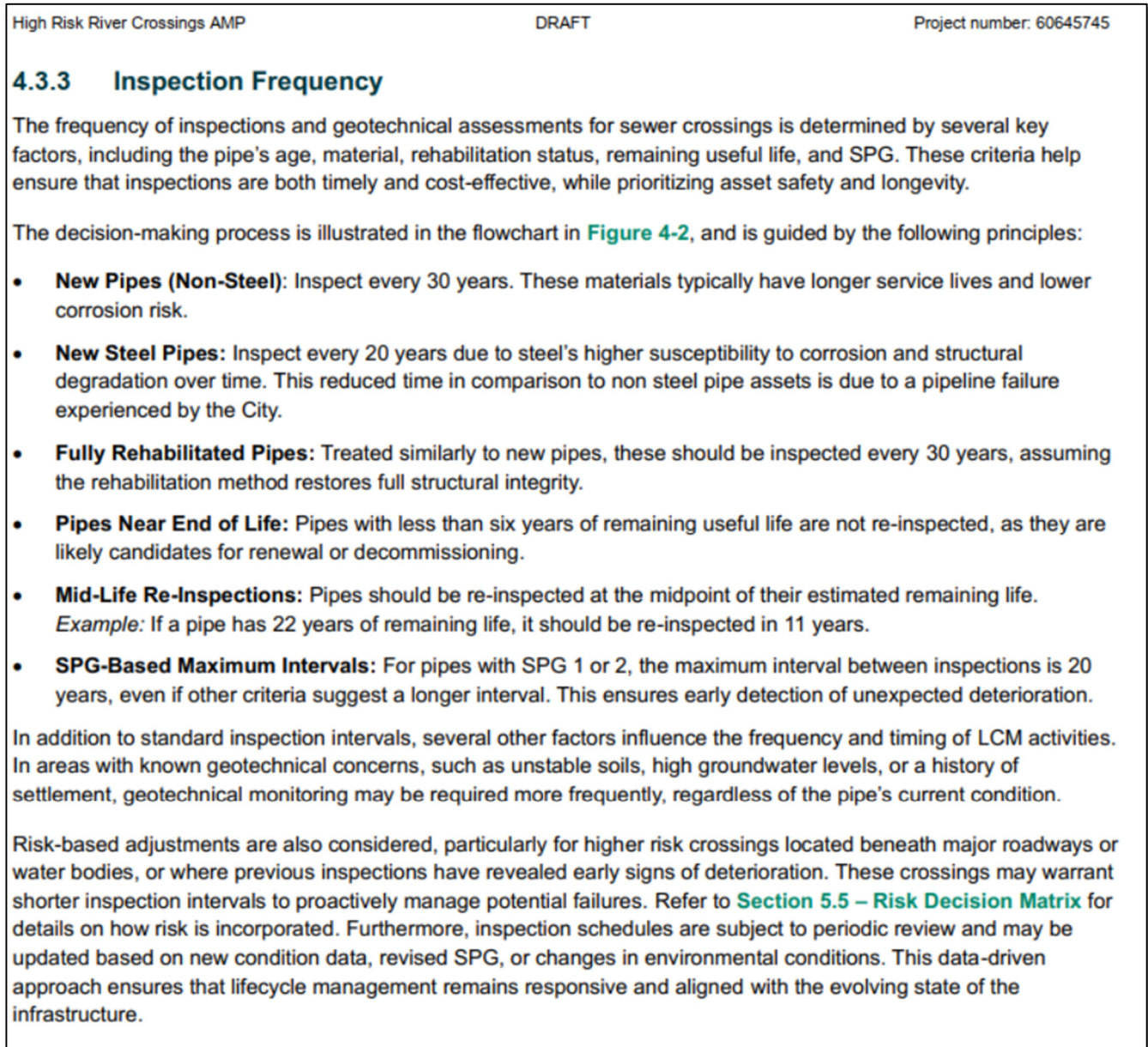


Figure 1: High Risk River Crossing AMP – Section 4.3.3 continued...

